

OCRM

HT
168

E94

E9

1989

JAN 21 1990

TITLE: EVERETT'S HARBORFRONT PUBLIC ACCESS CAPITAL IMPROVEMENT PLAN

AUTHORS:

Makers et. al.

Reviewed by Port of Everett and City of Everett

SUBJECT:

Shoreline Master Program Public Access Plan

DATE:

June 30, 1989

Sponsored by the Washington State department of Ecology, the Port of Everett, and the City of Everett

Copies of the report may be reviewed at:

City of Everett
Planning and Community Development Department
3rd Floor, 2731 Wetmore Avenue
Everett, WA 98201

PROJECT NUMBER:

G0089040

U.S. DEPARTMENT OF COMMERCE NOAA
COASTAL SERVICES CENTER

2234 SOUTH HOBSON AVENUE
CHARLESTON, SC 29405-2413

LENGTH:

19 pages

ABSTRACT:

Of all the natural assets that a city can possess, none is more important than an active, accessible waterfront. A busy harbor is the springboard for industry and trade. An attractive shoreline is a special amenity drawing citizens for a variety of recreational pursuits. Views of the water or proximity to the shoreline assets enhance the quality of residential neighborhoods and the vitality of business districts. Shorelines also provide a wealth of wildlife habitats crucial to fishing industry and the environmental health of our region. Waterfront planning to enhance these functions is a critical aspect of any comprehensive civic improvement program.

Throughout Everett's history Port Gardner Harborfront has played a leading role in the City's life and commerce. Today, the activities and character of the waterfront continues to change as the city itself develops. New maritime activities such as the Navy Homeport are replacing traditional mill town industries. A key ingredient in this transition is the public's desire to use the shoreline for recreation and enjoyment. Public access to the shoreline is mandated by the Washington State Shoreline Management act and reflected in the City of Everett's Shoreline Management Master Program. A recent public survey by Everett

HT
168
E94
E9
1989

Property of CSC Library

Parks and Recreation showed that the city's citizens placed access to the water as their number one priority for recreational development. The challenge is to provide public access to the shoreline through the development of esplanades, waterfront parks, boating facilities, viewing areas, commercial activities and other attractions without conflicting with the changing needs of maritime industries.

This plan is a joint effort by the City of Everett and the Port of Everett with funding assistance from the State of Washington Department of Ecology. Both parties recognizing the benefits of greater public access to the urban shoreline, commissioned the study to set mutually agreed upon policies regarding access requirements and to resolve specific issues related to the Port's contribution to public access projects. The Port and other shoreline developers are required by the Shoreline Management Master Program to provide public access component for each major shoreline development. Without a comprehensive shoreline access plan, the Port has had no assurance that public access improvements would not conflict with the development of vital maritime industries. Also, a unified public improvement plan was necessary to insure that the individual projects work together to best provide for diverse public needs and utilize the harborfront's opportunities.

Finally, a plan is needed so that improvements by the City, the Port, and other participants could be implemented more effectively through timing, combining funds, and developing projects that support one another.

The purpose of this study, then, is to:

1. Develop a set of public access projects in sufficient detail to determine their feasibility and estimated budgets. This set of potential projects will be accomplished as part of City sponsored construction, funded by the Port and other developers on the shoreline as part of shoreline permit requirements or implemented with funds from grants or other sources.
2. Develop a comprehensive plan that links and organizes public access elements into a more usable system.
3. Set a strategy and implementation policies for public access requirements that offer predictability for waterfront developers a procedure to resolve capital improvement development issues as they arise.
4. Resolve current issues regarding requirements for development at specific sites.

To lead the planning process toward these objectives and insure that both the City's and the Port's interest were considered, a project committee was established that included staff persons from the City Planning Department, Parks and Recreation Department, Mayor's Office, and the Port of Everett. The committee directed planning activities through weekly meetings at which various policy options, planning concepts, and individual public access features were explored. In addition, the Mayor, City Council members, and Port commissioners were briefed periodically.

The principal product of this effort is an array of potential capital improvements projects to promote public access. The emphasis has been on cost-effective, achievable projects that can be accomplished during the next 5 to 10 years. It is envisioned that the projects may be implemented by a variety of different parties using different funding sources. Some will be required as on-site or off-site mitigation for large developments. Others may be accomplished through Parks Department funds, Port development, Everett Public Works improvements, or governmental and private grants. Because many of the projects will be tied to other development actions and a variety of funding sources, the projects are not prioritized. Instead, a procedure for determining which projects are accomplished as to fulfill the requirements of various shoreline permits as they arise. Recommendations for this as well as critical policies for shoreline access development are summarized under "Implementation Policies and Procedures."

SUMMARY ACCOUNT:

The following has been accomplished:

1. Preparation of background data and information including:

Review existing plans, permits, and documents on the Everett Harborfront area.

Evaluated these plans, permits, and documents and prepared a background analysis for review.

2. Outlined specific public access improvements along the Everett Harborfront. These improvements include 1) fishing pier; 2) access road improvements including pedestrian/bikeway improvements; 3) viewing areas; 4) parking; and 5) restrooms. Improvements at each site will vary, but signs, street furniture, parking and landscaping and other aesthetic improvements will be provided at each site. The plan shows how the various sites are linked together for pedestrians and bicycles.

Developed conceptual site designs for the public access improvements. The conceptual designs will set the basic framework for the types of improvements and their locations along the waterfront. Final designs may be different but specific use improvements and site locations will remain as stated in the Plan when adapted.

3. Developed a phasing scheme for the public access improvements based on the timing of the development on the harborfront as it develops over time.

Note: Modifications may be made to the Plan and the phasing of the projects amended if circumstances arise which call for modification or changes in the Plan provisions. Detailed procedures for handling departures from the Plan will be outlined in the Plan.

4. A camera ready copy of the Plan was developed. A copy of the report is attached.

150d

Everett Harborfront Public Access Capital Improvements Plan

6-23-09
Preliminary Draft for Review

Everett Harborfront Public Access Capital Improvements Plan

INTRODUCTION

Of all the natural assets that a city can possess, none is more important than an active, accessible waterfront. A busy harbor is the springboard for industry and trade. An attractive shoreline is a special amenity drawing citizens for a variety of recreational pursuits. Views of the water or proximity to the shoreline assets enhance the quality of residential neighborhoods and the vitality of business districts. Shorelines also provide a wealth of wildlife habitats crucial to fishing industry and the environmental health of our region. Waterfront planning to enhance these functions is a critical aspect of any comprehensive civic improvement program.

Throughout Everett's history, Port Gardner Harborfront has played a leading role in the City's life and commerce. Today, the activities and character of the waterfront continues to change as the city itself develops. New maritime activities such as the Navy Homeport are replacing traditional mill town industries. A key ingredient in this transition is the public's desire to use the shoreline for recreation and enjoyment. Public access to the shoreline is mandated by the Washington State Shoreline Management Act and reflected in the City of Everett's Shoreline Management Master Program. A recent public survey by Everett Parks and Recreation showed that the city's citizens placed access to the water as their number one priority for recreational development. The challenge is to provide public access to the shoreline through the development of esplanades, waterfront parks, boating facilities, viewing areas, commercial activities and other attractions without conflicting with the changing needs of maritime industries.

BACKGROUND AND PURPOSE

This plan is a joint effort by the City of Everett and the Port of Everett. Both parties, recognizing the benefits of greater public access to the urban shoreline, commissioned the study to set mutually agreed upon policies regarding access requirements and to resolve specific issues related to the Port's contribution to public access projects. The Port and other shoreline developers are required by the Shoreline Management Master Program to provide public access component for each major shoreline development. Without a comprehensive shoreline access plan, the Port has had no assurance that public access improvements would not conflict with the development of vital maritime industries. Also, a unified public improvement plan was necessary to insure that the individual projects work together to best provide for diverse public needs and utilize the harborfront's opportunities.

Finally, a plan is needed so that improvements by the City, the Port and other participants could be implemented more effectively through timing, combining funds and developing projects that support one another.

The purpose of this study, then, is to:

1. Develop a set of public access projects in sufficient detail to determine their feasibility and estimated budgets. This set of potential projects will be accomplished as part of City sponsored construction, funded by the Port and other developers on the shoreline as part of shoreline permit requirements or implemented with funds from grants or other sources.
2. Develop a comprehensive plan that links and organizes public access elements into a more useable system.

3. Set a strategy and implementation policies for public access requirements that offer predictability for waterfront developers a procedure to resolve capital improvement development issues as they arise.
4. Resolve current issues regarding requirements for development at specific sites.

To lead the planning process toward these objectives and insure that both the City's and the Port's interest were considered a project committee was established that included staff persons from the City Planning Department, Parks and Recreation Department, Mayor's Office and the Port of Everett. The committee directed planning activities through weekly meetings at which various policy options, planning concepts and individual public access features were explored. In addition, the Mayor, City Council members and Port commissioners were briefed periodically.

CONTENTS AND ORGANIZATION

The principal product of this effort is an array of potential capital improvements projects to promote public access. The emphasis has been on cost-effective, achievable projects that can be accomplished during the next 5 to 10 years. It is envisioned that the projects may be implemented by a variety of different parties using different funding sources. Some will be required as on-site or off-site mitigation for large developments. Others may be accomplished through Parks Department funds, Port development, Everett Public Works improvements or governmental and private grants. Because many of the projects will be tied to other development actions and a variety of funding sources, the projects are not prioritized. Instead, a procedure for determining which projects are accomplished as to fulfill the requirements of various shoreline permits as they arise. Recommendations for this as well as critical policies for shoreline access development are summarized under "Implementation Policies and Procedures."

Even though the described projects will be developed individually, it is critical that they work together as a system. The key planning considerations in developing this comprehensive system are described below.

COMPREHENSIVE ACCESS PLANNING CONSIDERATIONS

A principal finding of the Department of Ecology's Urban Waterfront Policy Analysis (1998) is that individual shoreline access projects are most effective when part of a comprehensive public access plan. Although the focus of this report is on the individual elements, the committee continuously kept a broad, comprehensive vision in mind as they explored the opportunities for various projects. The key planning considerations in this vision are outlined below and illustrated on the Comprehensive Access Planning Issues map.

Provide a Variety of Activities and Settings

People use the shoreline for many different recreational purposes including swimming, boating, strolling, cycling, picnicking, wildlife watching and numerous others. Everett's harborfront, more than most other urban shorelines in Puget Sound, includes a variety of physical settings ranging from high intensity industrial facilities and pedestrian oriented commercial areas to open beaches and secluded wildlife habitats. A primary objective of this plan is to build on Everett's diversity, introducing new recreation and commercial activities and maintaining the unique environmental characteristics of the harborfront's several sub areas. Specific actions to increase variety are:

- o Develop a new car-top boat launch near the north marina
- o Maintain the natural setting of north Jetty Island
- o Add features to the Marina Village area including touch tanks, an interpretive arched and a view tower.
- o Construct a fishing pier

Link Activities and Features

Research indicates that shoreline parks and recreational elements are used more when they are connected by pathways that allow visitors to walk, jog, or cycle from one attraction to another. The increased popularity of exercise has produced an increased emphasis in "linear parks" which follow the shoreline for a considerable distance. Experience at Ruston Way (Tacoma), Myrtle Edwards Park and the Burke Gilman Trail (Seattle), has shown that such pathways can dramatically increase park usage and help to revitalize portions of the city.

Connecting important features along Everett's Harborfront is another key ingredient of this plan, especially since the section of shoreline considered here is over 6 1/2 miles long. It is also important to note that the harborfront is only a segment in the long range plan to establish a waterfront trail linking recreation, and civic resources all around the Everett Peninsula and connecting with the regional multi-use trail system. Equally significant is the opportunity to establish greater ties between downtown and the waterfront.

The pathway system recommended in this plan, is intended to accommodate walkers, joggers, skaters, and cyclists, although cycling is inappropriate in some of the high activity areas. Each of these modes has different needs and optimum travel distances. Some recreational walkers will travel up to several miles, however, a more typical walking trips will be under about half a mile and take about 10 minutes. More importantly, walkers avoid long stretches of monotonous conditions and prefer routes where the scenery or view changes at least every 500 feet (approx), or where there is a lot of activity. Joggers and skaters tend to favor routes that range from one to five miles and avoid congested activity centers. Cyclists prefer longer routes with three to twenty mile trips being more typical.

Bike trail ridership increases substantially with the length of the trail network.
On the other hand cyclists cannot travel safely in congested pedestrian areas. Where substantial numbers of cyclists and pedestrians are likely to share the same path, the minimum trail sections should be two, 6 ft wide lanes.

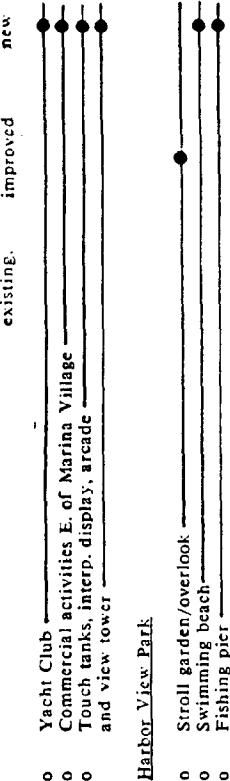
Some sections of pedestrian bicycle trail network connecting shoreline access features make use of established trails and others will be constructed as part of ongoing projects. Several projects to link existing sections are recommended in this plan.

Cluster Activities Where Possible

Another lesson learned from a review of successful waterfront parks is that clusters of several different activities will attract a lot of visitors. The region's most popular waterfront recreation facilities feature a wide range of compatible uses including active open space, eating, boating facilities, swimming beaches, gardens, views of maritime industry and wildlife and ships. To create mixed activities focal points, the plan calls for introduction new features and features into the compact pedestrian oriented centers.

The key activities for each of these centers is identified below and marks indicate whether the element is existing, will be improved or is a proposed new feature.

Activity Center	existing	improved	new
<u>Bike Launch Area</u>			
o Park and open space	●		
o Viewing pier	●		
o Pathway	●		
o Parking	●		
o Snack/bait shop	●		
o Office/restroom	●		
<u>North Marina</u>			
o Street-end park with kayak launch	●		
o Pathway on 13th Street	●		
o Marina esplanade	●		
o Overlook deck	●		
o Landscaping	●		
o Boating facilities	●		
<u>South Marina and Gilnet Fishing Area</u>			
o Fishing net repair area	●		
o Esplanade	●		
o Small park/deck	●		
o Viewing/fishing deck	●		



Foster an Appreciation for Maritime Activities, Local History, and the Natural Environment

Everett's waterfront has the potential to serve as a living lesson in maritime affairs. From several vantage points the visitor can view active maritime industries, naval activities, historic sites and numerous fascinating species of wildlife. Many people find the waterfront industrial and boating activities fascinating and facilities such as Seattle's Fishermen's Terminal and the Ballard Locks are popular attractions. An integrated mix of busy maritime activities and passive recreational and natural spaces is a cornerstone of this plan and it is important to allow people to safely experience to the variety that the harborfront affords. Several proposed projects will help visitors to better understand the historical, natural, and commercial importance of the harborfront the Everett community.

1. Interpretive displays at Legion Park that describe the historic Indian village and current activities below. Everett's Park and Recreation Department and City Library are currently installing interpretive signs which could be augmented with additional information, pavement and perhaps some lightings.
2. Nature walks in the north part of Jetty Island. The salt marsh east of marine View Drive and north of 10th Street is also an important habitat area and could be enhanced with a boardwalk.
3. Interpretive signage at key points along the harborfront pedestrian trail bicycle are recommended especially in the following areas:
 - o Pathway south of the boat launch expansion describing construction activities across the waterway.
 - o North marina esplanade emphasizing the history of the 13th Street mills. Enamel panels of photos of the mill sites would be popular attractions.
 - o A gallery of large marine large industrial artifacts at the park near the end of 13th Street. Everett has been a center of heavy marine construction and displaying examples of large buoys, tanks, propellers, etc., to help people experience up close the scale of such work.
 - o Gillnet Fishing Area. People will have a good opportunity to watch fishermen repair nets and outfit their boats so a few displays explaining the fishing activity (where they go, how they fish, etc.) would be appropriate.

o South Edge of Marina Village, "Touch Tanks" that allow visitors to hold invertebrate marine life, a tower to provide a view of the Navy Homeport and perhaps some working weather station apparatus, a display of environmental information, or Navy history would provide a real attraction at this spot.

o Bond Street Railroad Terminal. A view deck would allow excellent views of the log handling and Navy Homeport activities. A display could tell the story of logging operations and describe the Navy fleet.

o Water Department Reservoir. The rest stop along the bicycle trail is near the reservoir itself and is an ideal location for a display describing water department activities.

Ensure Visitor Safety

Active waterfronts can present numerous safety hazards and an important aspect of public access planning is reducing safety risks. Key principals that guided the overall planning and element design of this effort are:

1. Physically separate visitors from hazardous industrial situations. Fences and other barriers will be built to discourage traffic to the most hazardous sites such as South Terminal and the log handling yards.
2. Attract people to safe areas which provide views and other amenities. For example, the best views Gilnet fishing area will be from a convenient overlook above the net repair float. Also, the most direct pedestrian route will avoid the net rollers where fishermen back their trucks. Providing an attractive place to experience the activity reduces the chance that visitors will wander to where their presence could cause a conflict.
3. Separate pedestrian traffic from automobile traffic where possible.
4. Locate bicycle routes on less traveled streets. In cases where there is no opportunity to provide a bicycle route separate from traffic, the bicycle route follows less busy streets.
5. Provide Lighting. An amount for pedestrian lighting was included in the budgets for applicable elements.

Provide Adequate Parking

Most people will arrive to the harbor by automobile so parking is a major consideration. In some cases, such as the marina area and the Legion Park overlook, there already is parking. However, additional parking is recommended as noted in the individual descriptions. The estimated parking demand for new attractions should be refined as the individual elements are designed to insure that is adequate parking. One advantage of the mixed-use aspect of the plan is that peak parking demands for different activities occur at different times. (For example, office and industrial uses require work-day parking whereas recreational parking demand is higher or weekends. Also, by linking various activities with a convenient pedestrian walk, "point-source" parking demands can be spread over a greater area.

Everett Harborfront Access Elements

Listed below are the capital improvement elements recommended in this plan with construction budget estimate for each. These elements are located on the map on the following page and described in the attached design information sheets.

The construction budgets include taxes, design fees and contingency. The contingency and fees vary from element to element depending upon complexity of the work and the number of unknowns affecting the design.

harborfront chart

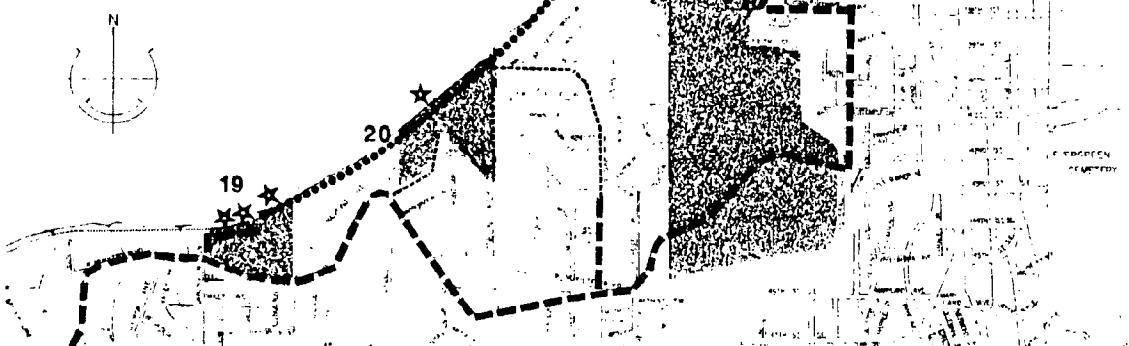
ELEMENT	COST	POTENTIAL CONTRIBUTORS							IMPLEMENTATION STRATEGY
		Port: Operational Improvements	Port: On-Site Mitigation	Port: Off-Site Mitigation	City of Everett	Developer Mitigation	Utilities	State & Federal	
1. Interpretive display Legion Park	\$ 2,000				●			●	
2. Boat launch - o Parking/festival site, park enhancement, launch enhancement & landscaping		●	●		●		●		
3. Streetscape and path on 10th Street	\$ 131,400		●		● ● ●				
4. Jetty Island o Dock o Shelters, beach enhancement & Nature Trail	\$ 310,000			● ● ●		● ●			
5. Bridge to Jetty Island	\$4,125,000	●	●	● ● ●	● ●	● ●	● ●		
6. Park at end of 13th Street with deck, boat ramp, parking & coord. w/future development	\$ 611,000	●	●		● ●		●		
7. Streetscape and path on 13th Street	\$ 693,000	●	●		● ● ●				
8. North Marina Esplanade improvements	\$ 448,000	●	●			●			
9. View decks at North Marina	\$ 187,800		●	●		●			
10. East Marina walk near fishing/boat building area	\$ 497,800	●	●						
14. 16th Street overpass and stairs from Grand Avenue Park	\$ 496,800		●	●	● ● ●	●			
15. Bicycle route signing on Grand south of Everett Avenue	\$ 4,000			●	● ●			●	
16. View deck at train station	\$ 54,000			●	● ●			●	
17. Bicycle trail near reservoir	\$ 63,000			●	●	● ● ●	●		
18. Rest stop at Forest Park and trail to beach	\$ 12,800			●	● ●	● ●	● ●		
19. Improvements at Harborview Park o Parking, landscaping and shoreside improvements o Pedestrian overpass & fishing pier	\$ 532,251 \$1,551,490			● ● ● ●	● ● ●	● ● ●	● ●		
20. Beach walk between Howarth & Harborview Parks	N/A								
21. Signage & misc. site elements	\$ 10,000		●	●	● ●			●	

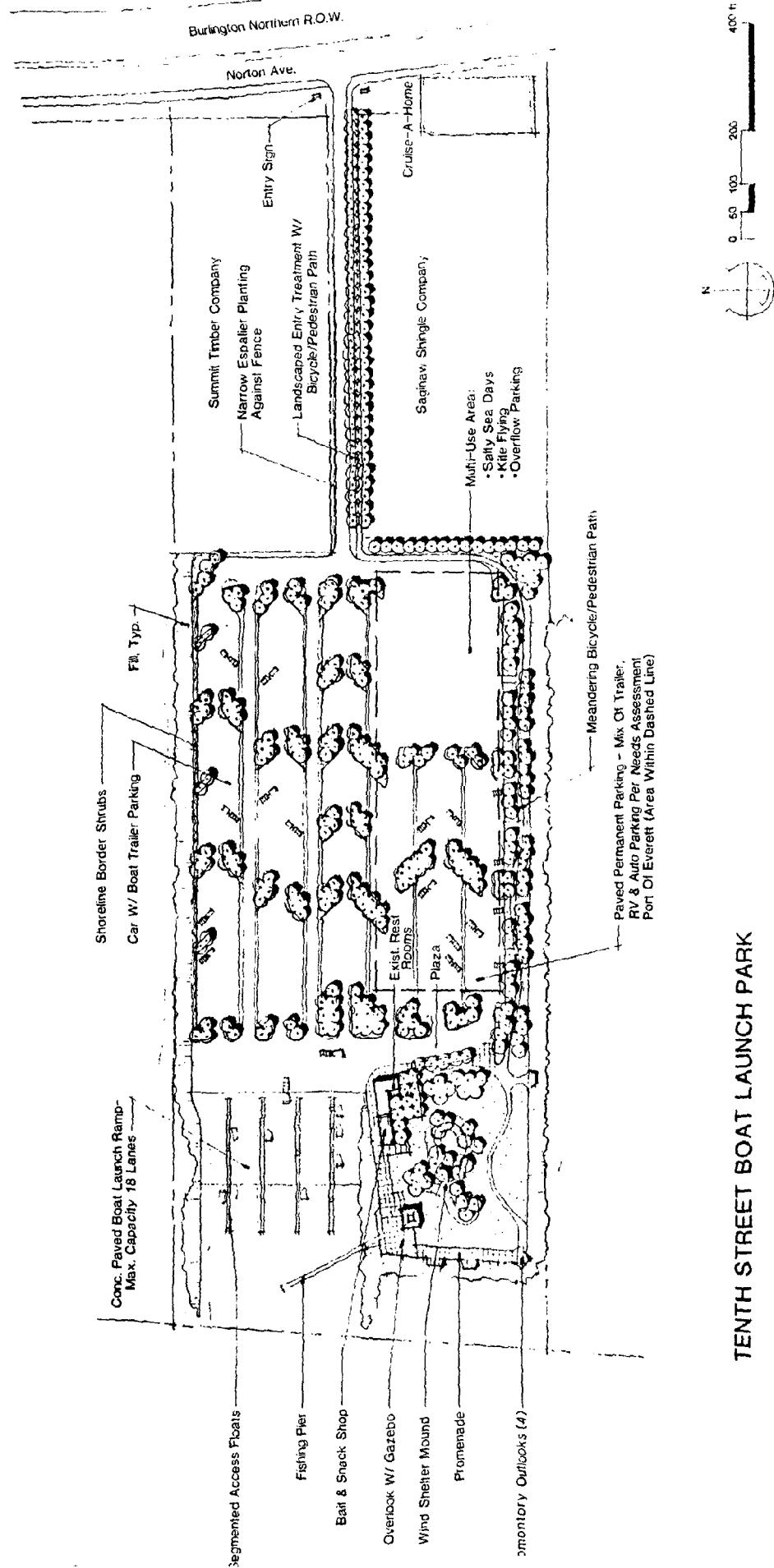
Everett Harborfront Public Access Capital Improvement Plan

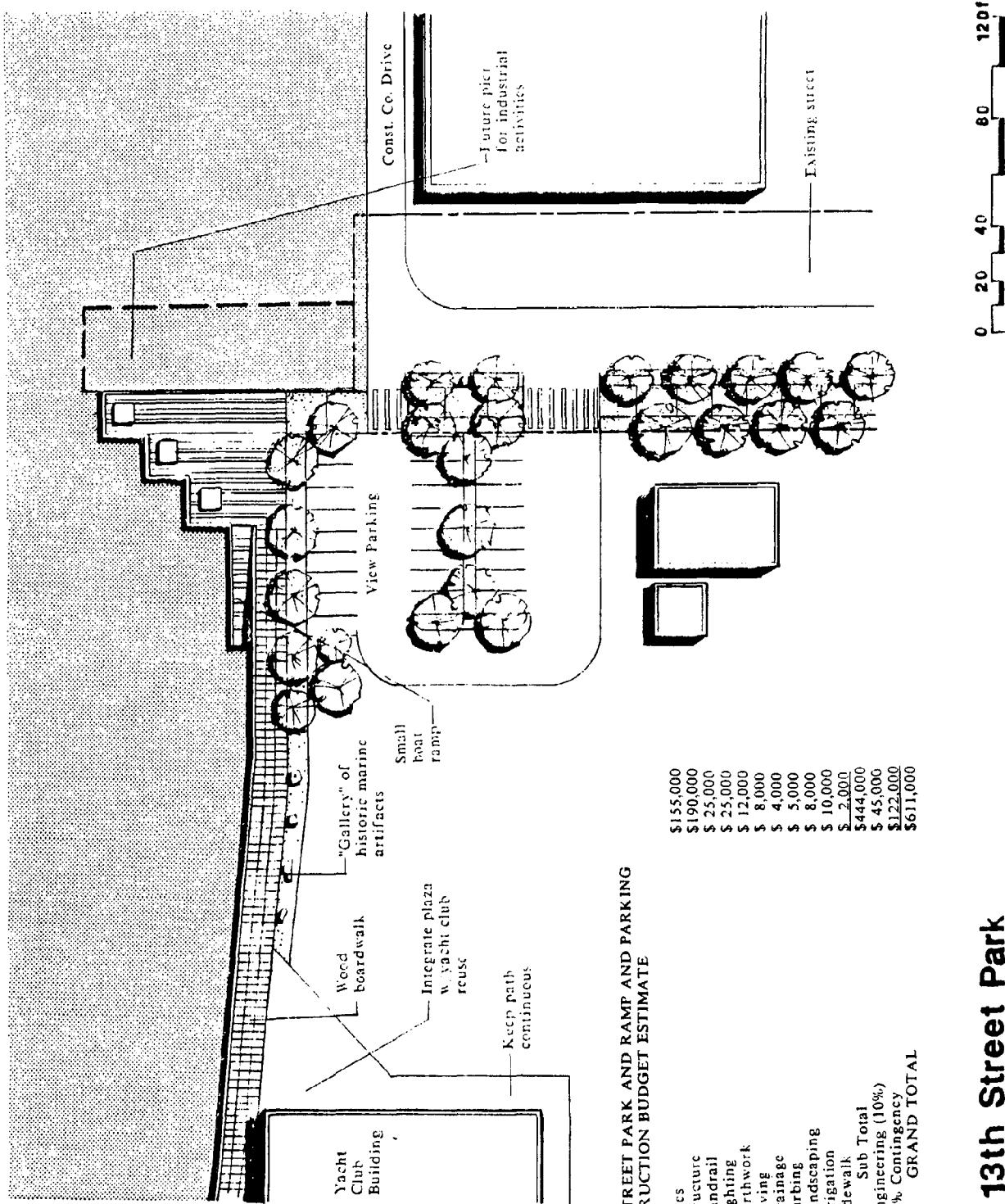
1. Viewpoint at Legion Park with interpretive display
2. Boat Launch Site: Improvements include parking, festival site, park enhancement, launch enlargement and landscaping
3. Streetscape improvements and path on 10th Street
4. Jetty Island: Improvements include: dock, shelter, beach enhancement and nature trail
5. Bridge or tram to Jetty Island
6. Park at end of 13th Street with view, dock, boat ramp and parking. Coordinate design, w/future development
7. Streetscape improvements and path on 13th Street
8. North Marina esplanade improvements
9. Overlook decks at North Marina
10. East Marina walk near fishing/boat building area
11. Pedestrian improvements at gillnet fishing area including net repair float, plaza transient moorage and esplanade extension. Coordinate with Pioneer Plaza development.
12. Marina Esplanade improvements
13. Esplanade terminus and Marina Village area improvements including touch tanks, view tower, and interpretive displays in an arcade enclosure
14. 16th Street overpass with staircase from Grand Avenue Park
15. Bicycle route signing on Grand Ave south of Everett Avenue
16. View deck at train station
17. Bicycle trail near reservoir
18. Rest stop at Forest Park and trail to beach
19. Improvements at Harborview Park including parking, landscaping, restrooms, pedestrian overpass and fishing pier
20. Beach walk between Howarth & Harborview Parks

Legend

- Park or open space
- Bicycle/ walking path
- Alternate trail route to special features
- Foot walking trail
- Special feature







13TH STREET PARK AND RAMP AND PARKING CONSTRUCTION BUDGET ESTIMATE

A. Piles	\$155,000
B. Structure	\$190,000
C. Handrail	\$25,000
D. Lighting	\$25,000
E. Earthwork	\$12,000
F. Paving	\$8,000
G. Drainage	\$4,000
H. Curbing	\$5,000
I. Landscaping	\$8,000
J. Irrigation	\$10,000
K. Sidewalk	\$2,000
Sub Total	<u>\$444,000</u>
Engineering (10%)	\$45,000
25% Contingency	\$112,000
GRAND TOTAL	\$661,000

13th Street Park

Boat Launch Improvements and 10th Street Path and Streetscape Improvements

Jetty Island

BACKGROUND AND DESIGN CONSIDERATIONS

The boat launch and adjacent park is owned by the Port and is a popular attraction. On summer weekends the launch itself is at capacity and the park is alive with sunbathers, kite flyers and picnickers. Existing restrooms, breakwater and an office complement the primary activities. The Port is planning to add another ramp and several new acres of parking to the facility. This plan recommends additional improvements be added to make the site more attractive and add to the recreational opportunities. They are:

- o Construct a 12' wide pedestrian bicycle path along 10th Street to connect to the Marine View Drive route. Install a row of street trees, and landscaping. This will significantly upgrade the entrance to the launch and there is sufficient increased right-of-way. A large decorative entrance sign should also be considered to further enhance the launch's visibility.
- o Construct new paved parking area with provision for the Salty Sea Days festival and a pathway connection around the south side of the lot. The Port will determine the number and type of stalls that are required (boat trailer vs auto), and there may be the opportunity for recreational vehicle facilities. The pathway should allow views of the industrial waterway and, where appropriate, interpretive displays of the activities that take place there. New and existing parking lots should be landscaped as indicated, however, a portion of the new lot should be left without poles or trees to accommodate large rides and tents during Salty Sea Days.
- o Enlarge the Boat Launch Park. This is already a popular attraction and additional space will increase its capacity. Small extensions of the marginal pathway over the water's edge are recommended. The park's open landscape design treatment should be extended to the new portion with some earth mounding for better views. Wind breaks should be provided around picnic areas either through low walls or dense landscaping. A small bait/snack/boating convenience shop will be a revenue generating amenity and provide a needed service.
- o Plan for the possibility of a tram or bridge to Jetty Island. The most logical location for a future bridge or tram system to Jetty Island is the southern edge of the new parking area. Because of the height needed for vessel clearance under a bridge or tram, the structures would need to extend eastward from the water's edge. While there is no guarantee that a bridge or tram will be funded within the next decade the recommended schematic site plan locates new structures to avoid potential conflicts with future tram or bridge construction.

BACKGROUND

Over the years many uses have been suggested for Jetty Island including a larger industrial terminal connected by a roadway from near the Alverson Bridge. Everett Parks and Recreation Department have developed a conceptual master plan for the area which recommends that the northern portions of island remain in a natural condition to serve as a wildlife habitat refuge and the southern portion of the island be gradually enhanced for swimming, sunbathing and other informal recreational uses. In the summer, there is a sight seeing boat that regularly ferry's passengers across the bay and the Port has built a new dock in the island for both passengers and individual recreational boats. Several improvement projects are recommended.

- o A nature trail to the less sensitive portions of the island's north end. Interpretive displays will explain the importance of habitat areas and help visitors identify wildlife species.
- o A boardwalk or trail to the beach areas.
- o Picnic shelters to provide shade and weather protection. Everett Parks and Recreation prototype picnic structures should be sited to fit into the island setting. Landscaping, low walls, or berms should be used to provide wind protection.
- o Portable restrooms can be used for short term needs, however, as the popularity of the beaches increases water, sewage, electricity, telephones and permanent restrooms will be required. A small concession stand may be a desirable service some time in the future.

ACCESS TO JETTY ISLAND

Lying 400 yards off of Everett's central waterfront, Jetty Island has the potential to be one of Puget Sound's premier recreational beaches. As an island it has the disadvantage of direct access. On the other hand, the island's isolation is also part of its allure. The current boat service to the island is limited by operating times and capacity, and it has been suggested that aerial gondola tramway be installed to carry visitors across the water. A pedestrian bridge has also been proposed. Either system must also allow shipping traffic in the channel and therefore be elevated 90° in the air. Both options would cost in excess of \$3 million to implement.

- o most logical location for a future bridge or tram system to Jetty Island is the southern edge of the new parking area. Because of the height needed for vessel clearance under a bridge or tram, the structures would need to extend eastward from the water's edge. While there is no guarantee that a bridge or tram will be funded within the next decade the recommended schematic site plan locates new structures to avoid potential conflicts with future tram or bridge construction.

North Marina Esplanade

BACKGROUND

The Port of Everett is undertaking a conceptual masterplan for the north marina area which will outline improvements to the marina and boating facilities, utility construction and the ultimate redevelopment of several parcels. Since these plans are currently in flux, the recommended access improvements focus on the marginal walk. Enhancing this esplanade will be compatible with all options being considered by the Port and will serve as a continuous spine to connect and support future development as it occurs.

DESIGN CONSIDERATION

The current esplanade consists of an 8' wide concrete panel walkway and a 4 foot landscape/utility strip that is in poor condition. Several alternative schemes for upgrading this walkway were explored, including extending the walk with a wooden boardwalk cantilevered over the water. The preferred solution achieves the desired 12' wide walking surface by installing pavers over the landscape strip and constructing a curb and gutter along the frontage road. This provides an opportunity to install street trees and a space for marina site fixtures such as waste receptacles, lighting and utility connections. Other features and design considerations include:

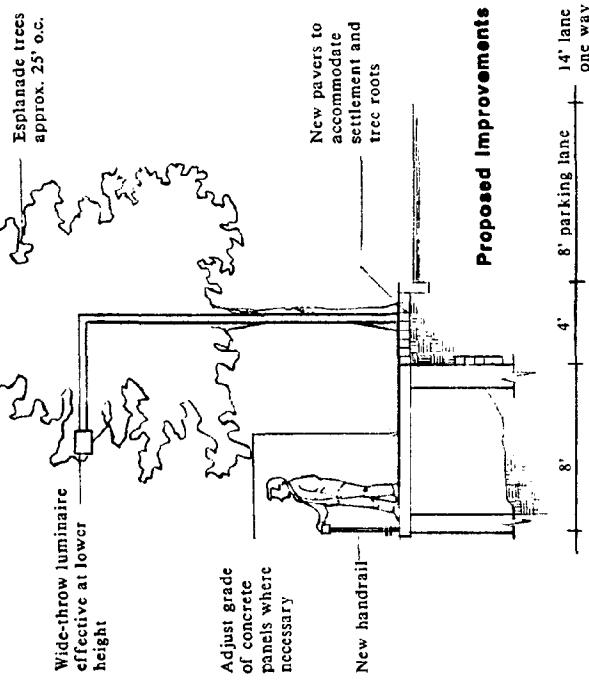
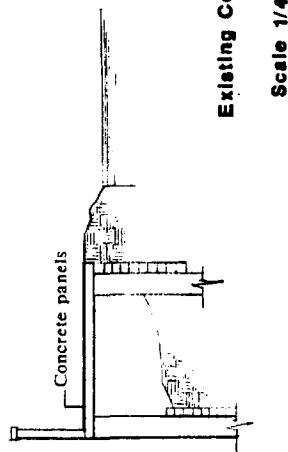
- o A new hand rail. A design similar to the south Marina would help to tie the two areas together visually.
- o New lights. There are two recommended approaches to lighting design. one would be to use the source lights on the south marina which would have the advantage of standardization. Smaller scaled light fixtures such as Sterner "mold cast" system which can be effectively mounted on a shorter post would be more appropriately scaled.
- o The street trees should be hardy for the conditions in the North Marina.
- o Interlock pavers by Weston would be a good choice for the unit pavers. Match the paving details of South Marina where appropriate.

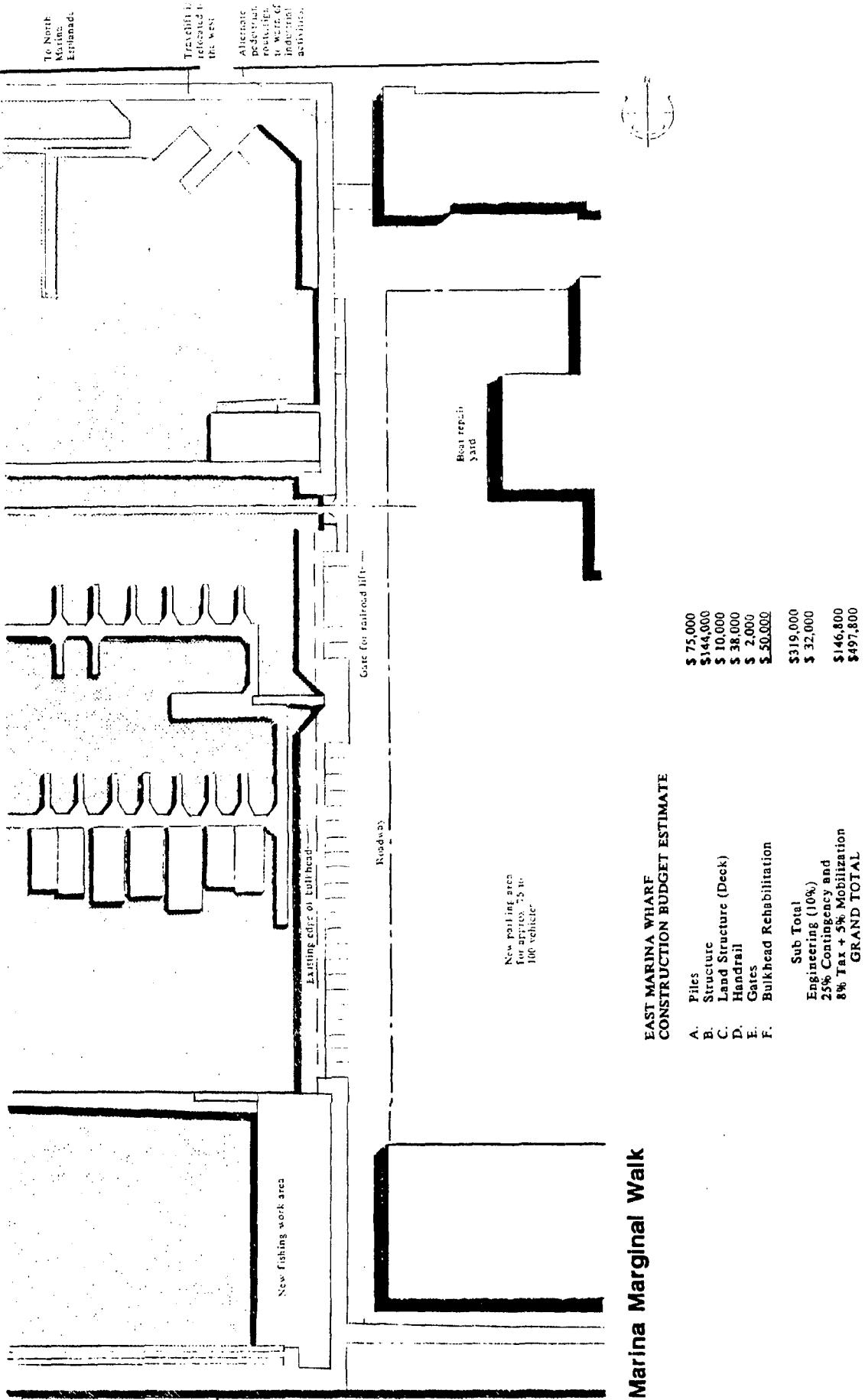
IMPLEMENTATION

Since this project directly supports the Port's North Marina activities and will enhance future development, its funding should not be considered as off site mitigation. The most likely and cost effective implementation will be part of the Port's North Marina Capital Improvement.

NORTH ESPLANADE IMPROVEMENTS CONSTRUCTION BUDGET ESTIMATE

A.	Handrail	\$111,000
B.	Brick Pavers	\$ 46,000
C.	Earthwork	\$ 7,000
D.	Trees	\$ 16,000
E.	Tree Crates	\$ 27,000
F.	Irrigation	\$ 14,000
G.	Lighting	\$ 45,000
H.	Curbing	\$ 14,000
I.	Patch Pavnt.	\$ 7,000
	Sub Total	\$287,000
	Engineering (10%)	\$ 29,000
	(25% Contingency, 8% tax and	\$132,000
	5% Mobilization)	
	GRAND TOTAL	\$448,000



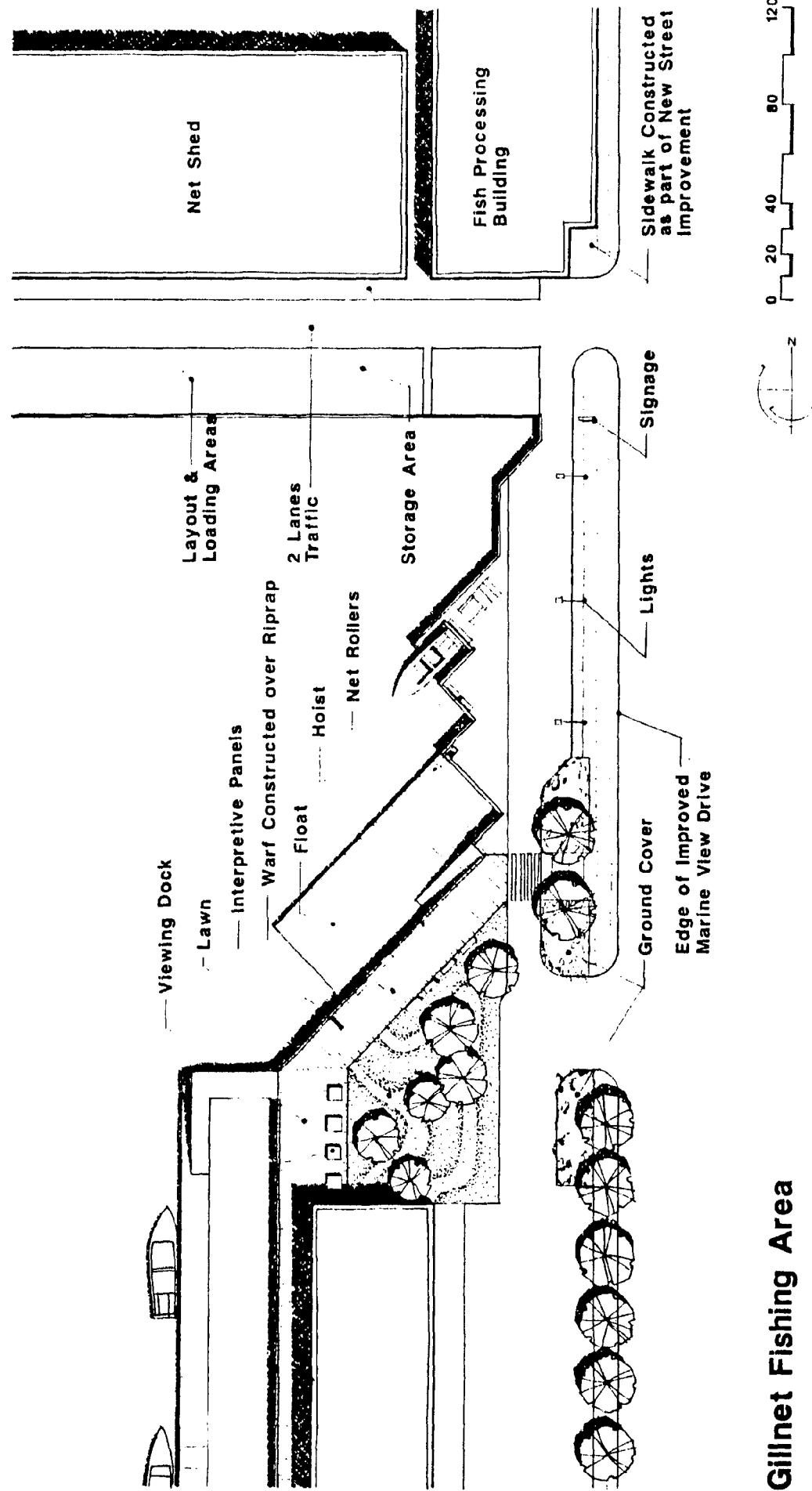


East Marina Marginal Walk

EAST MARINA WHARF CONSTRUCTION BUDGET ESTIMATE

A. Piles	\$ 75,000
B. Structure	\$144,000
C. Land Structure (Deck)	\$ 10,000
D. Handrail	\$ 38,000
E. Gates	\$ 2,000
F. Bulkhead Rehabilitation	\$ 50,000
Sub Total	\$319,000
Engineering (10%)	\$ 32,000
25% Contingency and	\$146,800
8% Tax + 3% Mobilization	\$497,800
GRAND TOTAL	

Gillnet Fishing Area



Gillnet Fishing Area

BACKGROUND

The Gillnet Fishing dock lies between the proposed Pioneer Plaza office complex to the south and the fish processing/boat building/gillnet seining fishing area to the north. The area currently supports net repair and loading of the purse serving fishing boats and the dock is a popular spot for mid-winter herring fishing. Widening of Marine View Drive will reduce the area for parking and necessitates reconfiguring of the existing fishing vessel dock. The proposed configuration is for two net rollers and a hoist for loading nets onto pick-up trucks and one net repair float. Truck access to the area is constrained but there will be room for one-way traffic entering from Marine View drive near Pioneer Plaza. Fishing activities will be visible from the plaza area just above the net repair float. The pedestrian/bicycle path will follow the Marine View Drive sidewalk at the fishing area to avoid conflicts with truck traffic and net haul out. The net repair float will be available from herring fishing during the winter. A guest moorage float is planned along the shoreline west of the Pioneer Plaza as part of that development.

DESIGN CONCERNS

- o The official signed access corridor will follow north along Marine View Drive. Those visitors who want to visit the wider trail area along the east margin will be able to walk westward from Stearts' along the layout area. However, signage will discourage this route because there is no way to provide a pedestrian route through the industrial area. Visitors will be advised to use caution if taking the route into the industrial area.
- o No street trees are recommended in front of the fishing dock to allow a clear view of the waterway from Marine View Drive.

- o The pedestrian walk and plaza area should be designed to complement the Pioneer Plaza Complex and to continue the materials, lighting rail details, etc. of the south marine esplanade. Several picnic tables and benches should be provided. Interpretive panels will be installed that describe the activities of the purse seining fleet, similar to what has been done at Fishermen's Terminal in Seattle.

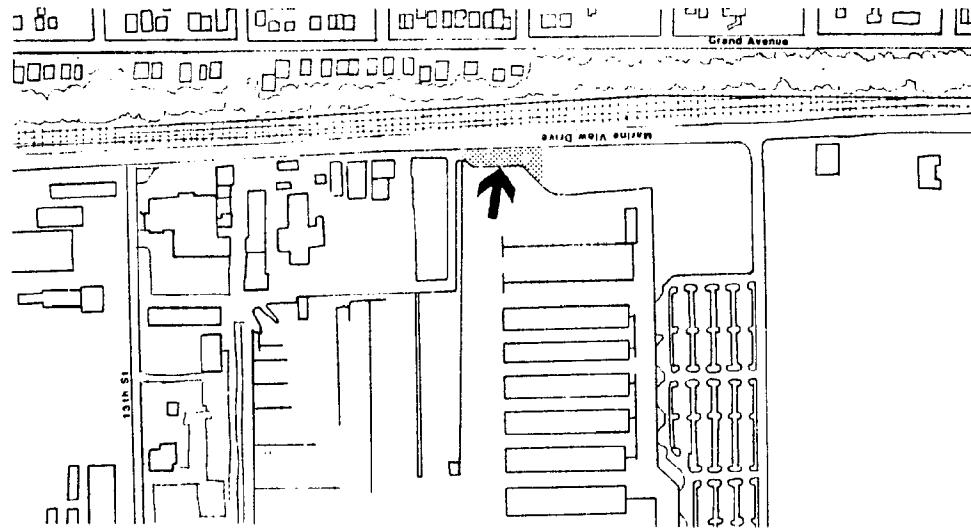
CONSTRUCTION BUDGET		LOCATION
	(in thousands)	
A. Timber Piles	\$113	
B. Wharf Structure	\$185	
C. Safety Ladders	\$5	
D. Gangway	\$15	
E. Gillnet Rollers	\$3	
F. Net Float	\$300	
G. Conc. Piles for Float	\$36	
H. Replace Exist. Wharf	\$266	
I. 2-Ton Crane	\$10	
J. Paving	\$3	
K. Sidewalk	\$9	
L. Curbing	\$3	
M. Landscaping	\$4	
N. Utilities	\$50	
Sub Total	\$1002	
Engineering (10%)	\$100	
25% Contingency	\$275	
GRAND TOTAL	\$1378	

OPTION FOR GILLNET WHARF: DO NOT REPLACE EXISTING WHARF

CONSTRUCTION BUDGET		LOCATION
	(in thousands)	
A. Timber Piles	\$113	
B. Wharf Structure	\$185	
C. Safety Ladders	\$5	
D. Gangway	\$15	
E. Gillnet Rollers	\$3	
F. Net Float	\$300	
G. Conc. Piles for Float	\$36	
H. Replace Exist. Wharf	\$266	
I. 2-Ton Crane	\$10	
J. Paving	\$3	
K. Sidewalk	\$9	
L. Curbing	\$3	
M. Landscaping	\$4	
N. Utilities	\$50	
Sub Total	\$1002	
Engineering (10%)	\$100	
25% Contingency	\$275	
GRAND TOTAL	\$1378	

FUNDING

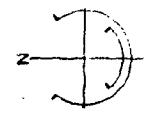
The fishing dock improvements including dock enlargement, net float and paving will be funded by the Port of Everett as part of their operational improvements. This will leave only minor elements including the signage and some landscaping (approximately \$2,000 for each) as part of on-site access mitigation to the fishing pier reconfiguration.



Existing Trees

Pedestrian Overpass

Pedestrian Stairs and Overpass near 16th St.



Grand Avenue Park

Concrete Steps &
Walk & Walk

Railroad Tracks

Marine View Drive

Pedestrian Walk

View Overlook

Gill Net Fishing
Area

Park

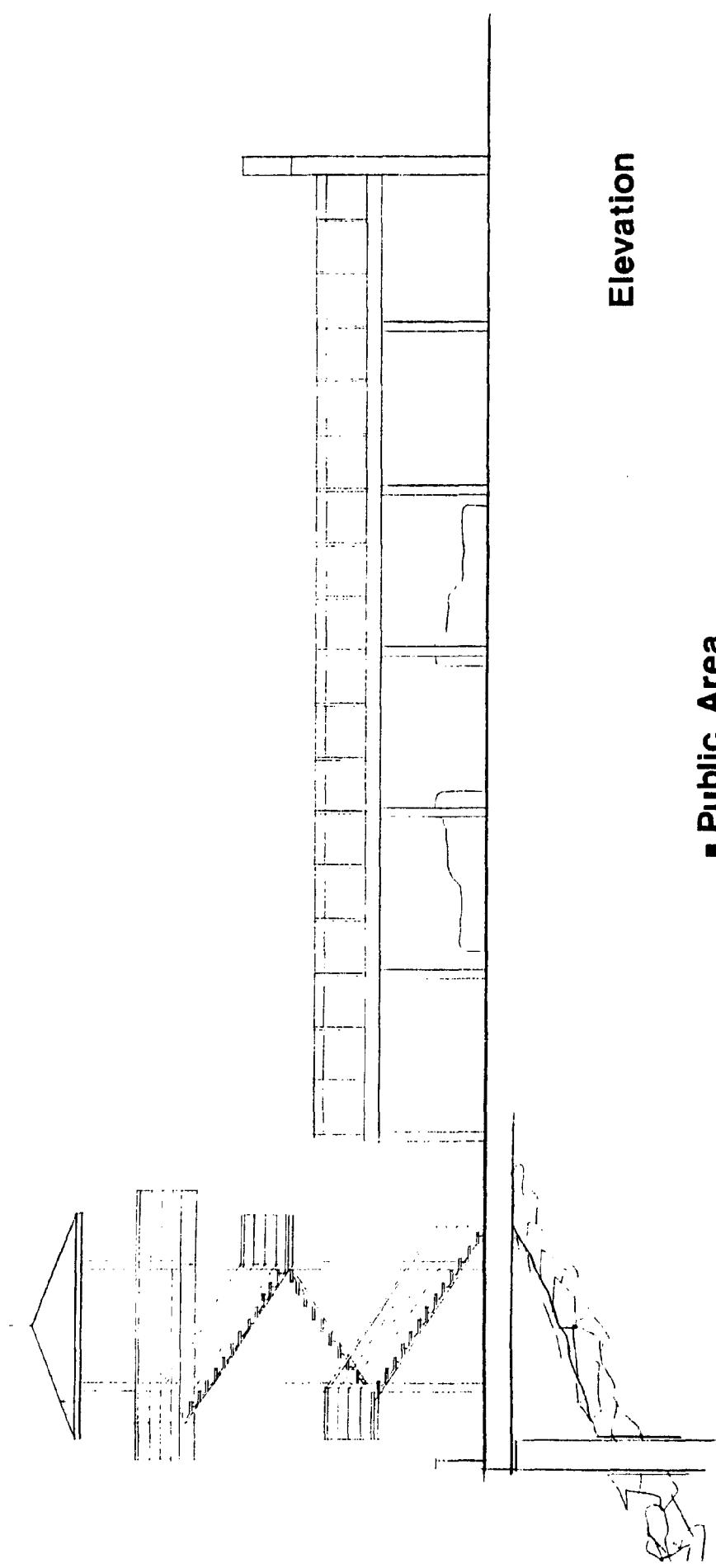
Pioneer Plaza
Development

Visiting Migrates

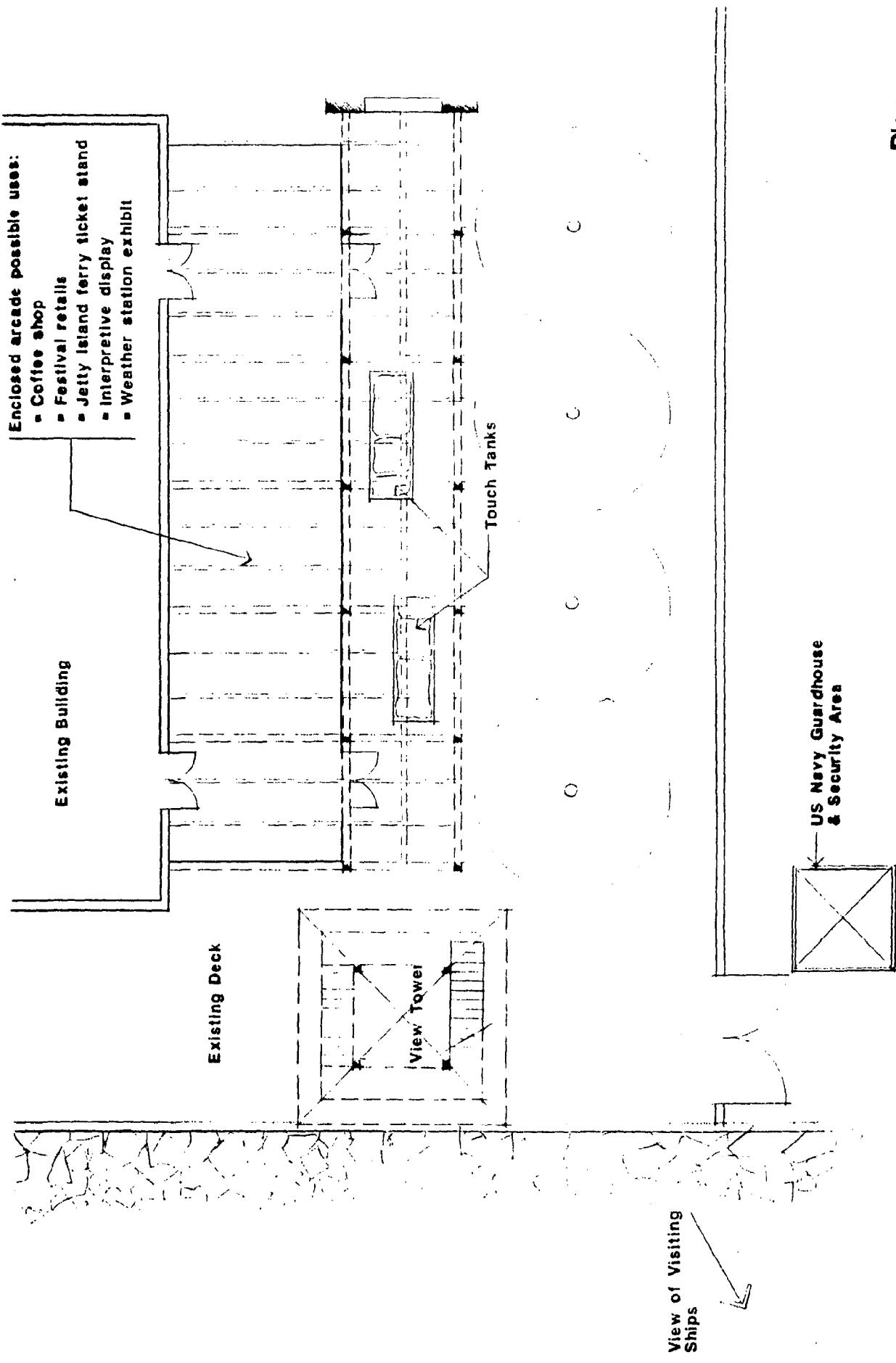
30ft
20
10
0

Elevation

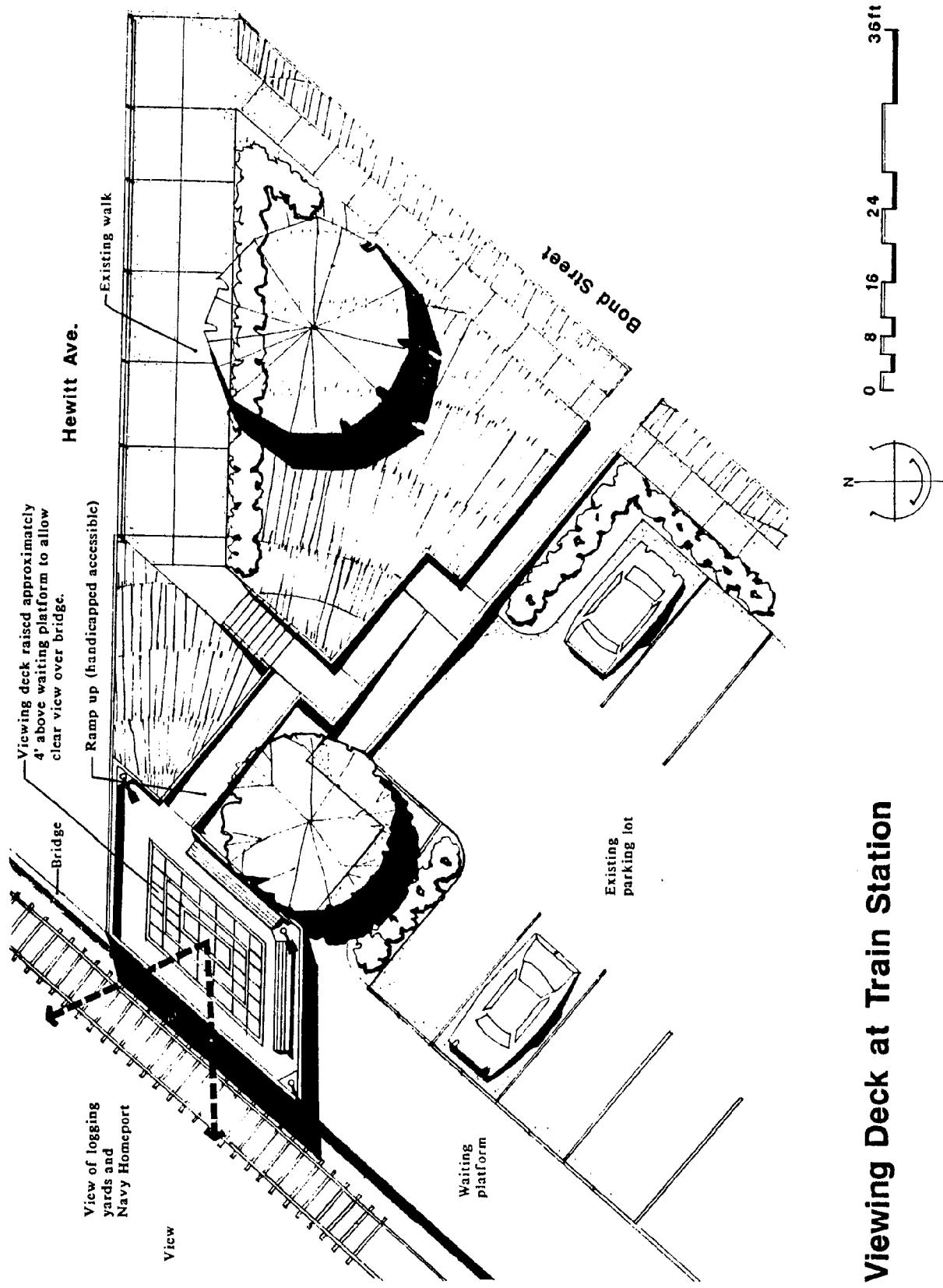
- Public Area
- View Tower
- Touch Tanks
- Marina Village Area Improvements



Plan



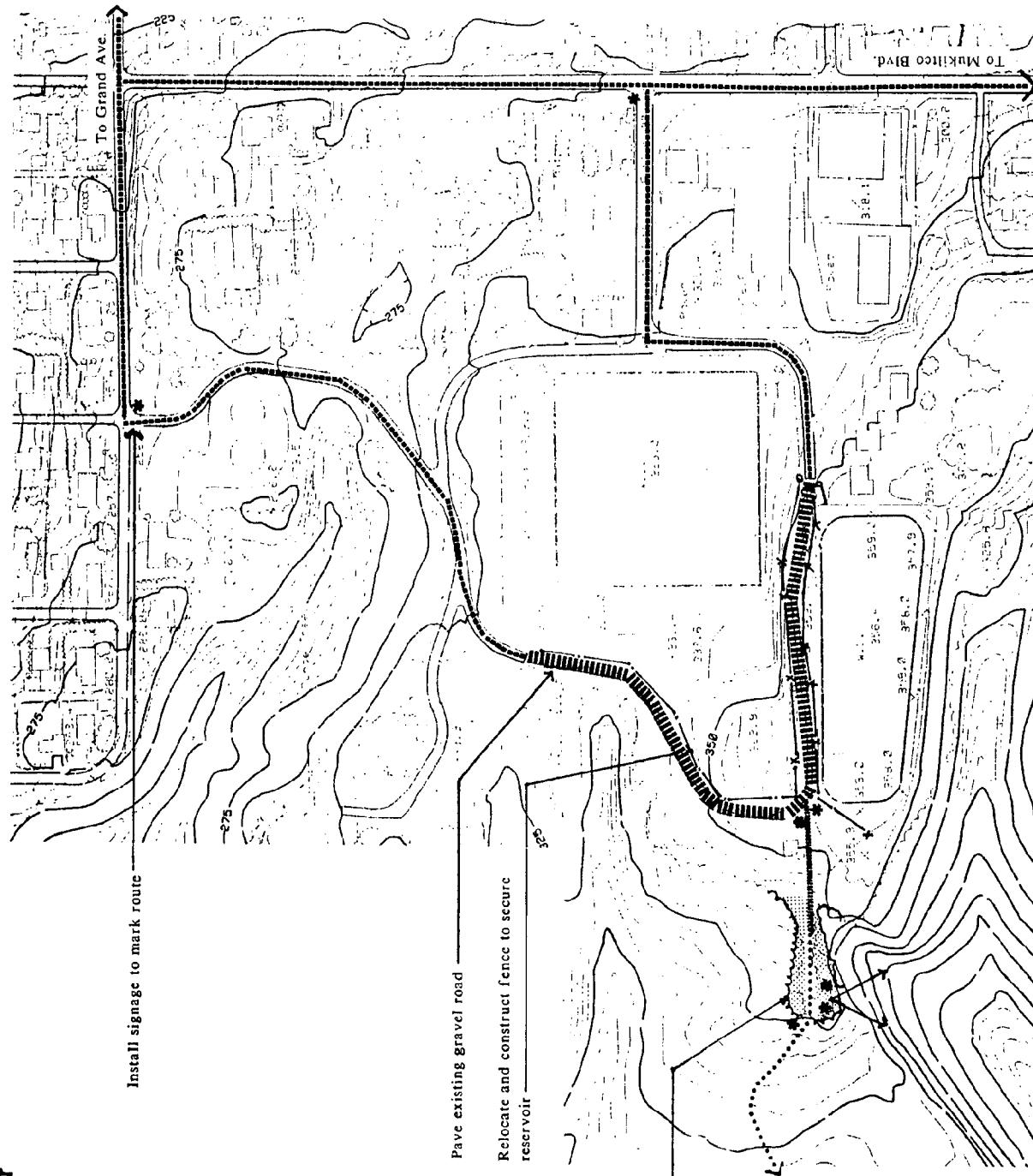
Viewing Deck at Train Station



Trail and Rest Stop Near Reservoir

LEGEND

- Signed bike route on street
- ||||| Paved path
- Gravel path
- * Signage or park furniture
- - - Relocated fence
- x- New fence



Bicycle Trail and Rest Stop near Reservoir

BACKGROUND

The bicycle route from Marine View Drive east on 25th then south through downtown and Grand Avenue. It then turns right and runs westward along 35th Street until it hits Federal. Cyclists at this point can conveniently bike down Federal to the bike lanes on Mukilteo Blvd. Those wishing a pleasant detour can continue westward along 35th to the Water Department Reservoir. Immediately to the east of the reservoir is an open field with the potential for excellent views of Pigeon Creek. Adding a few picnic tables and a bicycle rack would turn this corner of Forest Park into a pleasant and secluded rest stop. There is already a wooded foot path down to the salt water shoreline at the base of Pigeon Creek. The path goes near but not directly under the Pigeon Creek eagle's nest. Taken as a whole the rest stop trial to the water could be a significant park resource offering excellent views of several natural habitats and a quiet setting.

If the fence around the reservoir is reconfigured with new security gates and the existing reservoir loop could be achieved, allowing cyclists a convenient route back to Federal Avenue and then to Mukilteo Blvd. Another possible attraction would be the addition of a graphic display describing the water department operations and interesting facts about the city's water supply.

There are several parking stalls available at the west side of the reservoir so that the rest stop/pathway would be usable by non-cyclists. However, on street signage will be needed to direct people to the site because it is not visible from the roadway.

BUDGET ESTIMATE

Bicycle Trail Near Reservoir

Signage and interpretive display	\$ 5,000
Read/bike path (paving only 12' wide)	\$16,800
Relocate fencing at reservoir	\$11,200
New fencing at reservoir	\$ 9,000
4 gates	\$ 1,200
Raw Construction Cost	\$43,200

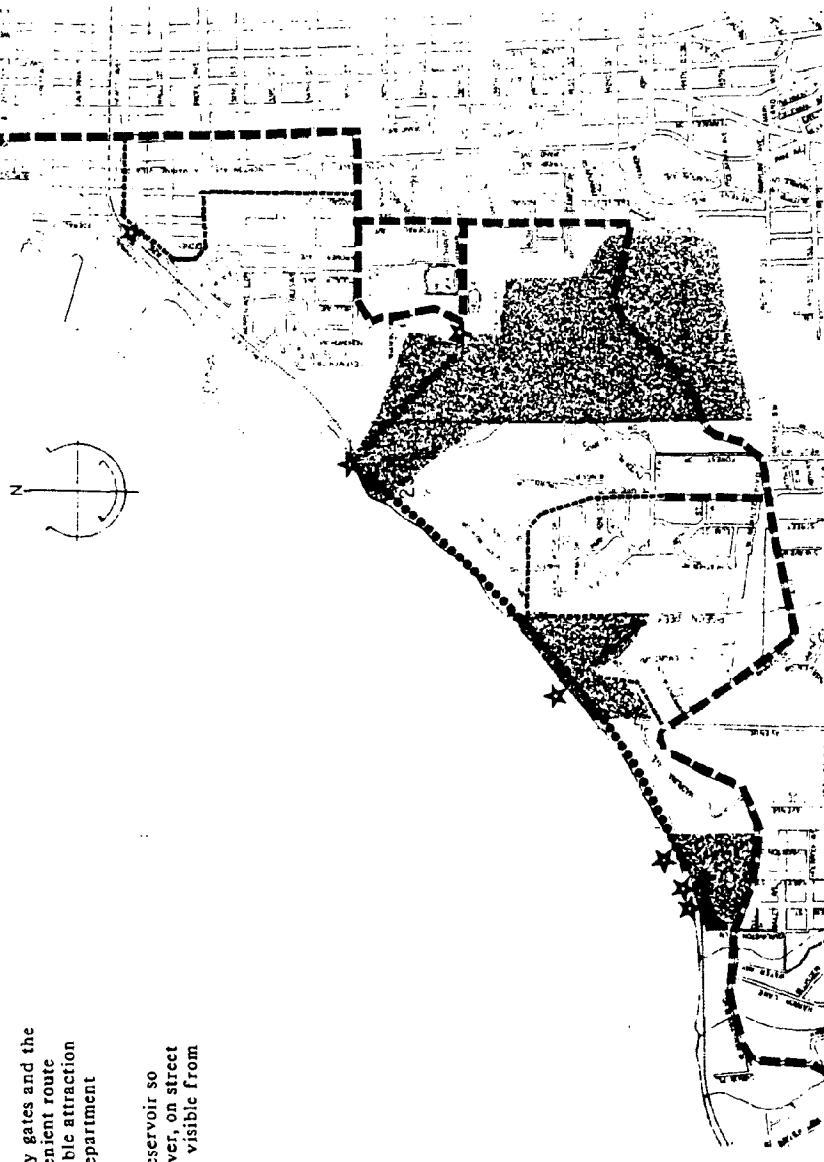
X 1.46 (taxes, fees, mobilization and contingencies) \$63,072

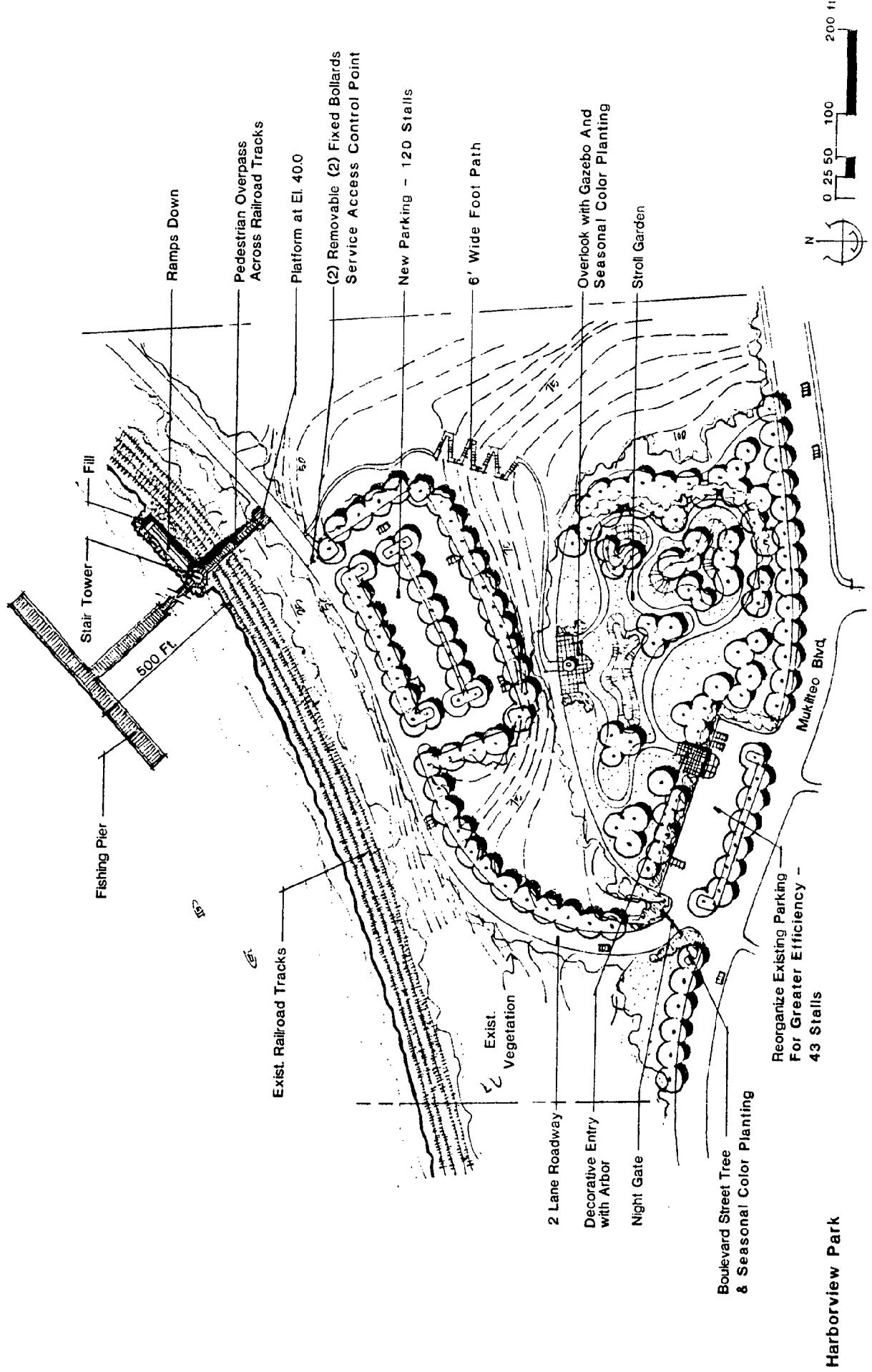
Rest Stop

4 picnic tables	\$ 2,232
Bicycle stand	\$ 765
Litter receptacles	\$ 500
Lighting	\$ 4,000
Foot path from reservoir to rest stop	\$ 1,250
Raw Construction Cost	\$ 8,747
X 1.46 (multiplier)	\$12,770

Legend

- Park or open space
- ■ ■ Bicycle / walking path
- Alternate trail route to special features
- Foot walking trail
- ★ Special feature





NOAA COASTAL SERVICES CENTER LIBRARY



3 6668 14108 0236